


485 Cooper Street, Epping Development Plan

21 June 2024

485 Cooper Street, Epping Development Plan

The Development Plan was approved by the City of Whittlesea on 16 July 2024, in accordance with Clause 43-04 Schedule 33 of the Whittlesea Planning Scheme.

25/07/2024..... .....
Signature of the Responsible Authority

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1 Introduction

The Development Plan has been prepared on behalf of The GPT Group in accordance with the requirements of Clause 43.04 (Development Plan Overlay – Schedule 33) of the Whittlesea Planning Scheme.

The purpose of the Development Plan is to provide an area-specific framework for the future development of 485 Cooper Street, Epping (the site). The site comprises a total area of approximately 352,600sqm and is made up of one (1) allotment, identified as Lot 1 on Plan of Subdivision 519650.

The Development Plan includes design objectives and necessary information to guide the assessment of any future planning permit applications.

1.1 Development Plan Overlay

This Development Plan has been prepared for a site within the Cooper Street South-West Employment Area. The key objectives for the employment area as prescribed through DPO33 include:

- *Activated frontages along Cooper Street, the Merri Creek and the Barry Road Grasslands.*
- *The prohibition of Restricted Retail uses along the Cooper Street frontage.*
- *Office uses oriented to the street frontage.*
- *Car parking areas to be located at the rear of buildings where possible, or sleeved to minimise their visibility from the street.*
- *Provision of landscaping within the frontage of the site and within car parking areas.*
- *Incorporation of existing natural features (including remnant vegetation) into the street layout and design response.*

- *Provision of a road along the eastern boundary of the development plan area, generally consistent with the existing carriage way easement in this location.*

Pursuant to the requirements of Schedule 33, Clause 3.0 of the Development Plan Overlay, the Development Plan has been informed by, and should be read in conjunction with the following specialist reports detailed as appendices on Page 21 of this document.

1.2 Purpose of the Development Plan

The purpose of the Development Plan is:

- *To implement the Municipal Planning Strategy and the Planning Policy Framework.*
- *To guide the form and conditions of future use and development on the site before a permit can be granted to use or develop the land.*
- *To exempt an application from notice and review.*
- *To respond to the relevant requirements of DPO33 - Cooper Street South-West Employment Area.*

1.3 Development Plan Context

The City of Whittlesea is a growth area municipality located on the metropolitan fringe approximately 18 kilometres north of the Melbourne CBD. With a population of 231,799 people (profile id) and a land area of approximately 490 square kilometres (profile id), the municipality is one of the largest and fastest growing in metropolitan Melbourne.

The municipality is made up of a diverse range of land use types and areas including established residential, industrial and commercial precincts. As noted, this Development Plan relates to land in the Cooper Street South-West Employment Area which is part of the broader Cooper Street

Employment Precinct. The precinct is predominantly made up of commercial and industrial land uses.

Cooper Street is the major east-west arterial road linking High Street to Sydney Road, intersecting the Hume Highway approximately halfway between the two. The precinct is well serviced by key transport infrastructure and connections including the Hume Freeway / Craigieburn Bypass, Western Ring Road, Melbourne Airport and Essendon Airport. The precinct will benefit from the future Outer Metropolitan Ring Road (OMR) and Beveridge Intermodal Freight Terminal (BIFT) which will be key pillars in Victoria's freight and logistics infrastructure.

The Development Plan area is bounded by the Merri Creek to the west, the Barry Road Grasslands and Merri Creek Park to the south, the Hume Freeway / Craigieburn Bypass to the east, and 481 Cooper Street to the North. Further east beyond the Hume Freeway the land has been developed for residential purposes.

No. 481 Cooper Street is undeveloped but is subject to a current planning process which includes the provision of the Development Plan to facilitate industrial development on the land. The land between 481 Cooper Street and the road has been developed for industrial purposes.

The development plan area is irregular in shape and is distinguished by significant level changes throughout, with a number of escarpments, undulations and former quarries. The site falls away sharply at its western boundary (Merri Creek interface). The site is also predominantly obscured from view of the Hume Highway due to a stand of trees beyond the site's eastern boundary and the undulating land.

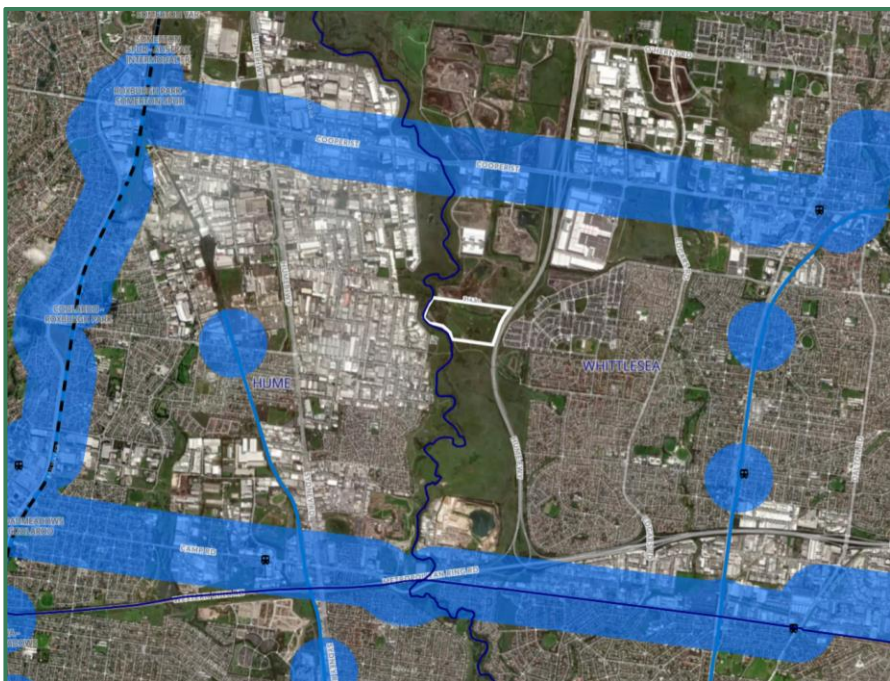


FIGURE 1 - THE DP AREA IN WHITE IS WELL SERVICE BY KEY TRANSPORT INFRASTRUCTURE AND WITHIN A PREDOMINANTLY INDUSTRIAL PRECINCT.

Please refer to the Planning Report prepared by *proUrban Advisory, Planning & Management* for further details of the Development Plan context.

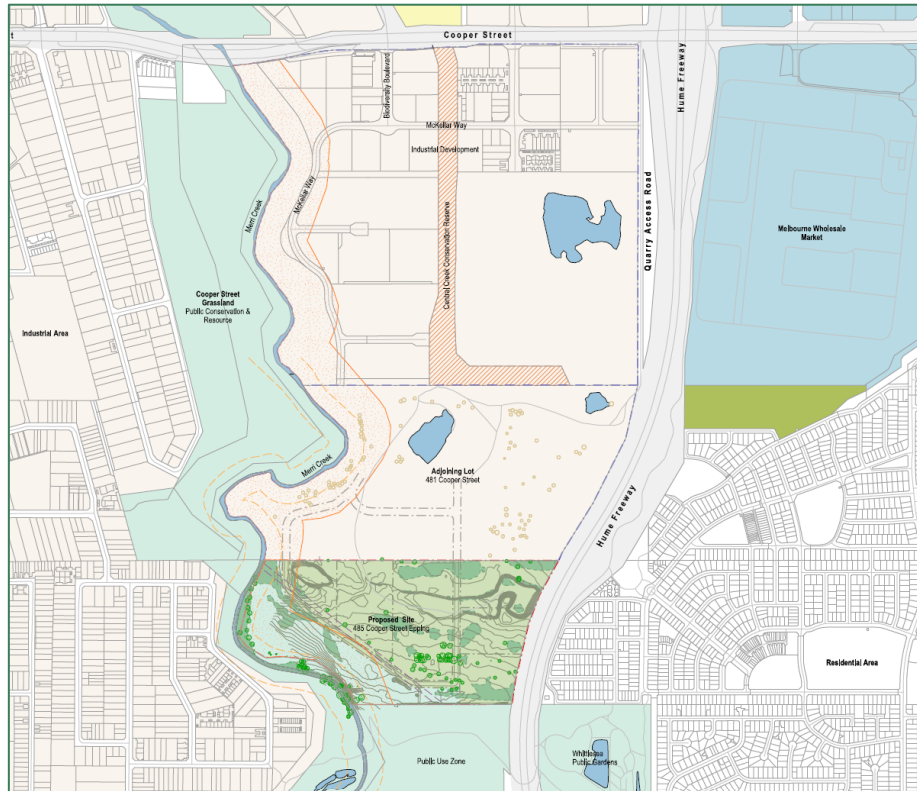


FIGURE 2 - SITE CONTEXT PLAN

2 Site Context

2.1 Planning Policy Framework

Zone & Overlays

The subject site is affected by the Industrial 1 Zone (IN1Z) and Urban Floodway Zone (UFZ) and the following overlays:

- Environmental Significance Overlay – Schedule 3 (ESO3)
- Design and Development Overlay – Schedule 2 (DDO2)
- Development Plan Overlay – Schedule 33 (DPO33)
- Melbourne Airport Environs Area Overlay – Schedule 2 (MAEO 2)
- Land Subject to Inundation Overlay (LSIO)

Further details of these planning controls are outlined within the Planning Report prepared by *proUrban Advisory, Planning & Management*.

Under the provisions of the Whittlesea Planning Scheme, the site is identified for industrial development. More specifically the following objective is sought:

- *To provide for manufacturing industry, the storage and distribution of goods and associated uses in a manner which does not affect the safety and amenity of local communities.*

Further information on the State and Local Planning Policy Framework and relevant particular provisions of the Planning Scheme are provided within the town Planning Report prepared by *proUrban Advisory, Planning & Management*.



FIGURE 3 - THE ZONES AFFECTING THE DP AND SURROUNDS.

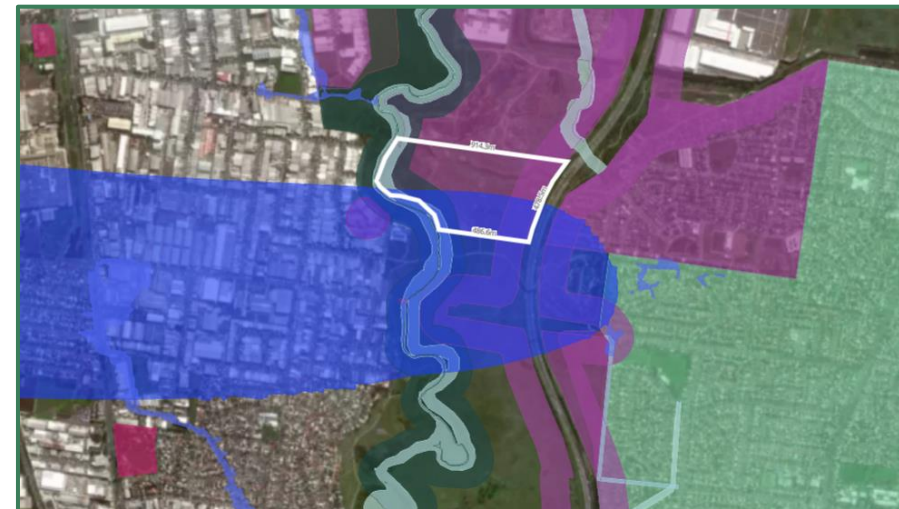


FIGURE 4 - PLANNING OVERLAYS AFFECTING THE DP AND SURROUNDS.

3 Development Plan

3.1 Overview of Development

The Development Plan seeks to provide developable area that will enable industrial development and employment opportunities consistent with the objectives of the Cooper Street South-West Employment Area. In addition to responding to the employment objectives, the Development Plan seeks the retention of a significant portion of vegetation along the Merri Creek interface. The vegetated area is proposed to be incorporated into the Merri Creek Park.

The proposal balances the competing priorities by enabling circa 231,602sqm of development area, 14,260 sqm of open space and 82,000 of habitat zone along Merri Creek.

This design will ensure the open space area is functional for both the flora and fauna that will exist on the site as well as the employee population that will utilise the space. Further, situating the open space and vegetation corridor in this location has the further benefit of integrating the estate with Merri Creek.

The open space and vegetation corridor along Merri Creek is proposed to be retained and enhanced, thus providing an opportunity to develop the balance of the site for industrial purposes that will respond to the employment and commercial objectives of the area.

3.2 Development Plan

The proposed Development Plan is shown in Figure 3 and is also incorporated as Appendix 1.

3.3 Site Staging Plan

Figure 1 also provides the indicative staging for the estate.

- Stage 1 will incorporate the delivery of the Stormwater Treatment, Conservation Zone and road infrastructure as well as the large centrally located development area.
- Stage 2 will incorporate development in the north-west and south-east.
- Stage 3 will incorporate development in the north-eastern portion of the site.

Refer to the Development Plan at Figure 5.

The staging of the site is contingent on the development of the site to the north.

3.4 Future Development Applications

As part of future planning permit application for subdivision and development, in addition to other requirements of the Whittlesea Planning Scheme, the following documentation is required to be prepared to the satisfaction of the responsible authority:

- Kangaroo Management Plan;
 - Before the certification of the plan of subdivision, a Kangaroo Management Plan must be approved by the responsible authority. The Kangaroo Management Plan must be submitted to the Secretary to the Department of Energy, Environment and Climate Change Action (DEECA) for review and the comments of DEECA are to be considered by Council in its assessment of the submitted Kangaroo Management Plan. The submitted Kangaroo Management Plan should include:
 - Strategies to avoid land locking kangaroos, including staging of subdivision.

- Strategies to minimise animal and human welfare risks.
 - Management and monitoring actions to sustainably manage a population of kangaroos within a suitable location.
 - Actions to address the containment of kangaroos and to ensure appropriate animal welfare.
 - The approved Kangaroo Management must be implemented to the satisfaction of the responsible authority.
- Tree and Native Vegetation Removal and Retention Plan;
- Conservation Management Plan:
 - Before the certification of any stage of subdivision the permit holder must prepare a Conservation Management Plan for the land. The Conservation Management Plan (CMP) must be prepared to the satisfaction of the responsible authority, must make reference to any Cultural Heritage Management Plan developed for the site, incorporate proposed open space in the marram baba Parklands corridor, include the partial retention of Patch A and include a Translocation Plan for Growling Grass Frogs and/or other species of significance.
 - The Conservation Management Plan is to be secured on title via a Section 173 Agreement of the Planning and Environment Act 1987.
 - Action items identified within the Conservation Management Plan must be completed to the satisfaction of the Responsible Authority prior to the issue of Statement of Compliance.
- A Translocation Plan for Growling Grass Frogs or other species of significance must be submitted to Melbourne Water for approval, in accordance with the draft Conservation Management Plan.

- Assessment of the waterbodies and their environs for fauna is a requirement prior to the commencement of subdivision with appropriate survey, salvage and translocation provisions by suitably qualified professionals.
- The ultimate location of the road adjacent to Merri Creek is subject to design and interface considerations, including impacts to native vegetation, at the planning permit stage.

As part of future planning permit application for use and buildings and works, in addition to other requirements of the Whittlesea Planning Scheme, the following documentation is required to be prepared to the satisfaction of the responsible authority:

- Waste Management Plan,
 - The Waste Management Plan must demonstrate how the capture, collection and disposal of recyclable and non-recyclable waste will occur.
- Construction Environmental Management Plan (including Asbestos Management Plan)
 - An asbestos management plan (AMP) in conjunction with, or separate to a Construction Environmental Management Plan (CEMP), is recommended to be developed prior to any significant earthworks on site. Based on historical and recent environmental investigations, the Site is considered suitable for commercial/industrial land use and the proposed development; acknowledging the implementation of industry best practice management plans addressing the Site conditions encountered to date.

The final Conservation Management Plan must be submitted to Melbourne Water with access gate design and location included.

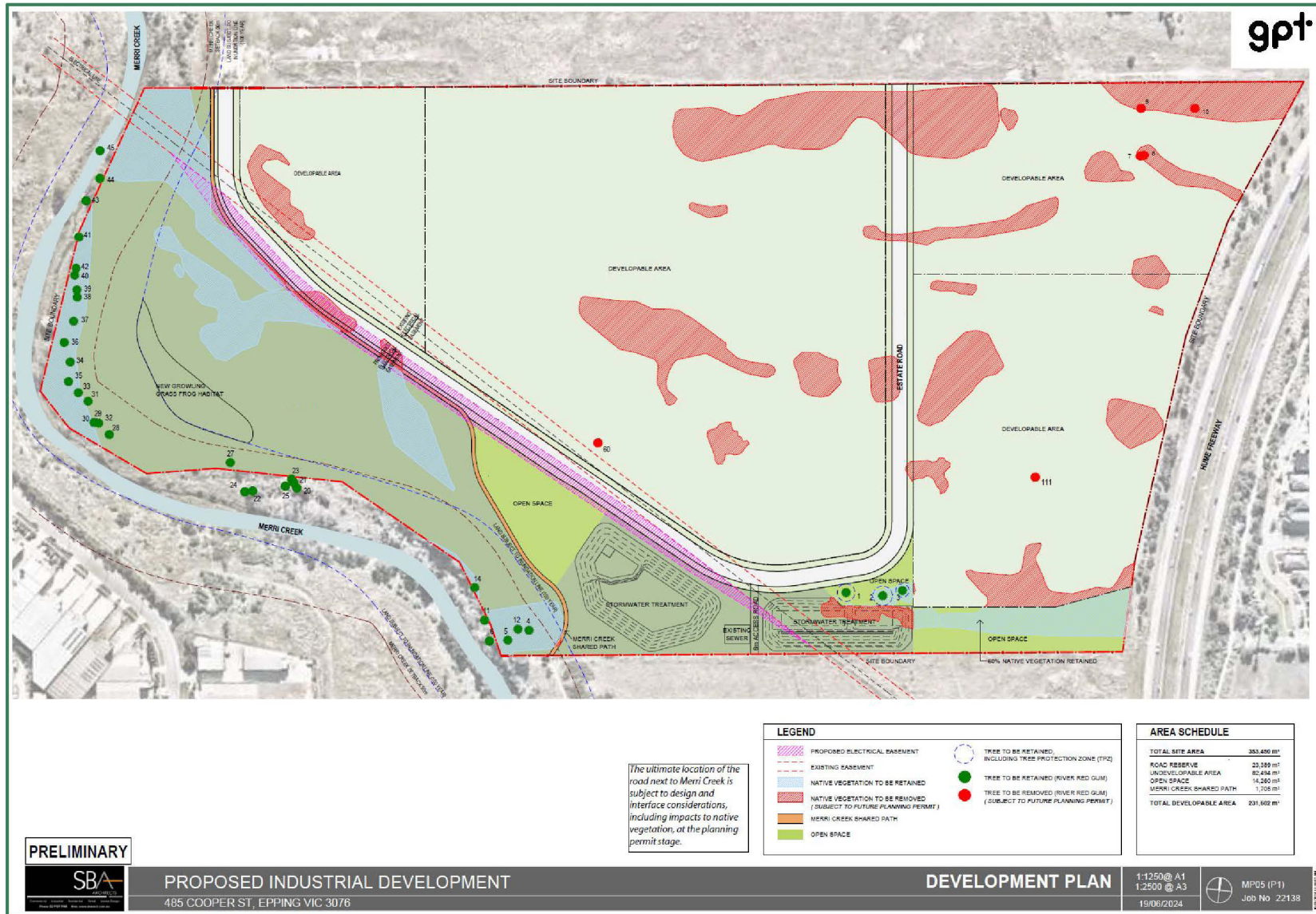


FIGURE 5 - DEVELOPMENT PLAN LAYOUT INCLUDING INDICATIVE STAGING

4 Design Guidelines

The Development Plan layout has been designed to achieve a high-quality industrial precinct that is environmentally sustainable, promotes visual amenity and appropriately integrates the open spaces and industrial areas. The following design guidelines will assist in ensuring that the future development of the site will be of a high quality.

Setbacks

- Building setbacks must allow for active street frontages and be landscaped to enhance the appearance of the development. Visitor car-parking including landscaping is encouraged near office entry.
- The front building setback is to be landscaped in accordance with City of Whittlesea's Landscape Guidelines for Industrial Development.
- All storage of goods and materials, refuse collection areas and garbage skips will be fully screened from view. Screening materials must be consistent with building and landscaping on site.
- Buildings are to be setback a minimum 6 metres from any boundary adjoining a conservation reserve, habitat corridor or road reserve abutting a conservation reserve/habitat corridor.
- Substations & services should be avoided within the front setback, and a landscape screen should be provided subject to approval from the relevant service provider.

Building Design

- Buildings are encouraged to be of high-quality design and visually attractive.
- Buildings and offices (or dock offices) should orientate themselves to address and provide surveillance of the road network and the parkland areas.
- Building facades of office developments are to be modulated and

articulated, which may include architecturally expressing structural elements of the building.

- The provision of articulation and use of glazing, height, variation in materials and textures and the use of architectural features and Ensure forms and surface treatment strategies is encouraged to provide visual interest.
- The visual impact of large wall expanses is to be reduced in scale by architectural treatment, landscaping and/or the use of colour.
- A dedicated separate 1.5m footpath must be provided from the street to the office entrance.
- Roof design and exposed structures including lift motor rooms, plant rooms, air conditioning, satellite dishes, ventilations and exhaust systems to be suitably screened, integrated and not detract from building appearance.
- Where buildings include side or rear elevations facing an open space or drainage reserve, they should be well articulated and provide a component of glazing for passive surveillance.
- Shadow diagrams must accompany any application for use or development of lots adjacent marram baba Parklands and galada tamboore Grasslands.
- There is a requirement that no buildings are constructed that would overshadow the Barry Road Grasslands during the hours of 10am – 3pm on 22 June.
- Recommendation to avoid overshadowing of habitat within 50 metres of a wetland or waterway.

Sustainable Design

- All development must have regard to City of Whittlesea's Environmentally Sustainable Development policy found within Clause 15.01-2L of the Whittlesea Planning Scheme. In addition, it is recommended that:
 - Solar Photovoltaic is encouraged for all developments.
 - Electric Vehicle parking space/s and associated charging

infrastructure to be encouraged within the estate.

- Buildings constructed to achieve a Solar Reflectance Index value of greater than 50 by avoiding dark roofs.
 - Consideration of external shading measures particularly to north and western glazing to reduce solar heat gain.
 - Encourage buildings to be designed to minimise embodied energy and operational energy consumption in order to reduce lifetime greenhouse gas emissions from the development.
- Maximise on-site infiltration including use of rainwater tanks to maximise rainwater collection and re-use opportunities. Encourage use of large smart rainwater tanks.
 - Permeable surfaces, landscaping and Water Sensitive Urban Design (WSUD) treatments should be considered rather than large expanses of hard surfaces.
 - Hard surfacing in landscaped areas such as walkways between car parking spaces is to be prioritised as permeable surface.
 - Energy-efficient lighting is encouraged to be used to reduce energy consumption and operating costs.

Fencing

- Fencing on the side and rear property boundary should be a black-coloured chain wire mesh or palisade fence to a maximum height of 2.1m.
- Maximise active frontages to the street and adjoining areas by providing architectural treatments when fencing is unavoidable.
- Basalt plinths, (or similar) end walls on corners is encouraged.
- Encourage fences and gates to be integral to the design of building proposed for the site.
- Security fencing should have a high degree of transparency and be located behind the landscape buffer. In the event fencing is required for security purposes for side and rear boundary it is to be plastic

coated and encouraged to be no more than two strands of barbed wire.

Uses

- A component of office should be orientated towards the primary street frontage to allow for a sense of address and activation of the street. Alternatively, the office areas should be orientated towards the on-site car parking areas to provide visual surveillance.
- Staff amenity area which includes private outdoor eating and sitting for staff is to be provided on each lot. This area is to have a minimum of 15m² and located away from intrusive sources.

Car Parking and loading facilities

- Car parking within the front setback should be minimised and preferably restricted to visitor parking and all abilities parking. Any car parking located in front of buildings must be adequately screened with landscaping.

Loading and Storage

- Wherever possible, loading facilities are to be located at the side or rear of developments to minimise their visual impact.
- All loading and unloading facilities shall be provided for within the site and screened to the satisfaction of the Responsible Authority.
- Where possible, loading and servicing areas should be designed as an integral part of the development on each site.
- All storage of goods and materials, refuse collection areas and garbage skips will be fully screened from view. Screening materials must be consistent with building and landscaping on site.

Access

- All vehicles, including those delivering to or servicing the site, must be able to enter and leave it in a forward direction.
- Ensure truck/heavy vehicles are able to safely egress the site and do not conflict with the pedestrian/walking environment.

Signage

- Advertising signage should form an integrated part of the building façade, architectural design and the scale of the building.
- The number of signs should be limited to avoid cluttering distraction and unnecessary repetition.
- Each development should provide appropriate directional signage to assist with the movement of pedestrian and vehicular traffic. Directional signage should be of a consistent type and style throughout the development. In general, this signage should explain:
 - Site entries and exits
 - Staff and visitor car parking
 - Goods delivery and pick-up
 - Reception/office areas

Waste Management

- All waste materials and refuse shall be stored in areas specifically designated for this purpose.
- Waste and refuse storage areas which are external to buildings on the site must be fully screened.
- Storage areas for waste materials and reuse should be an integral part of the design of the buildings.

Lighting

- All premises must provide external lighting to ensure adequate site security.
- All car parking areas should be provided with appropriate lighting to ensure safety and security of users after dark.
- All lighting must be located, directed and baffled to limit light spill beyond the site boundaries.

Maintenance

- All buildings must be maintained in good order and appearance.
- Sites that are not improved or built upon shall be maintained in a clean and neat appearance by the landowner.

Landscaping

- Landscape plans for each site must be prepared in accordance with the City of Whittlesea's Landscape Guidelines for Industrial Development.
- Species selection to favour hardy and drought tolerant species with low maintenance and low watering requirements. Species selection must preference local indigenous species.
- Species selection and maintenance schedules must consider the need to manage weeds, and the potential for exotic species to seed in nearby conservation areas.
- River red gums proposed for retention must form an integrated and protected part of development.
- The front building setback is to be landscaped to minimize the impact of built form and hardscaping as viewed from the public realm, conservation reserves and open space.
- In large car parking areas of 20 or more car spaces, garden beds with canopy trees, low shrubs and ground covers shall be provided at regular intervals of every 8 continuous car spaces.
- Locate substantial perimeter planting and landscaping around the borders of the estate.

Open Space and Conservation Reserves

- Open space provided within the Merri Creek Marram Baba Corridor must be designed to provide visitor infrastructure and a level of embellishment to the satisfaction of the Responsible Authority
- Appropriate embellishments including rest furniture are to be provided along the shared pathway and in open spaces along the Merri Creek Marram Baba regional parklands to respond to requirements of a regional parkland within the City of Whittlesea Open Space Strategy 2016.
- Shared pathways, facilities and open space provided within the Merri Creek Marram Baba corridor must be designed with safety and functionality at the core of design **and located to avoid impacting native vegetation.**

- Tree reserves are to be embellished with seating/rest furniture to encourage multi-purpose use of these reserves.
- Tree reserve boundaries and setbacks from significant trees for retention must be outside of the tree canopy and protection zones, in accordance with City of Whittlesea's Tree Protection Zone requirements.
- Tree reserves, conservation areas and open space where they interface with road reserves and lots must include vehicle exclusion fencing treatments that are visually permeable with pedestrian openings to the satisfaction of City of Whittlesea.

5 Access, Parking and Movement

The Transport Impact Assessment has considered the existing and proposed road network of the surrounding area. The following details are provided within Appendix 2:

- The location of existing roads and intersections;
- The location of future roads and intersections;
- The Principal Public Transport Network connections;
- Traffic management works, including intersection types;
- Indicative traffic generation rates for future land uses and its distribution within the road network; and
- Traffic volumes anticipated for the local road network and connections to the arterial road network, including identification of future upgrade works required.

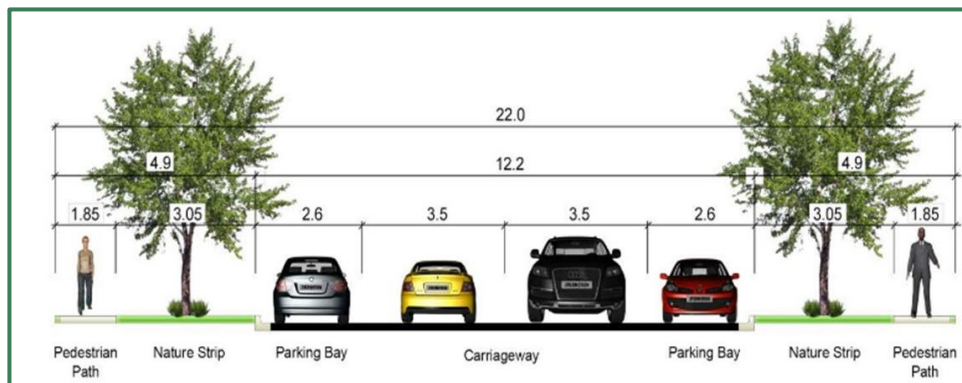


FIGURE 6 – STANDARD ROAD CROSS SECTION

5.1 Proposed Path Network and Industrial Development Plan

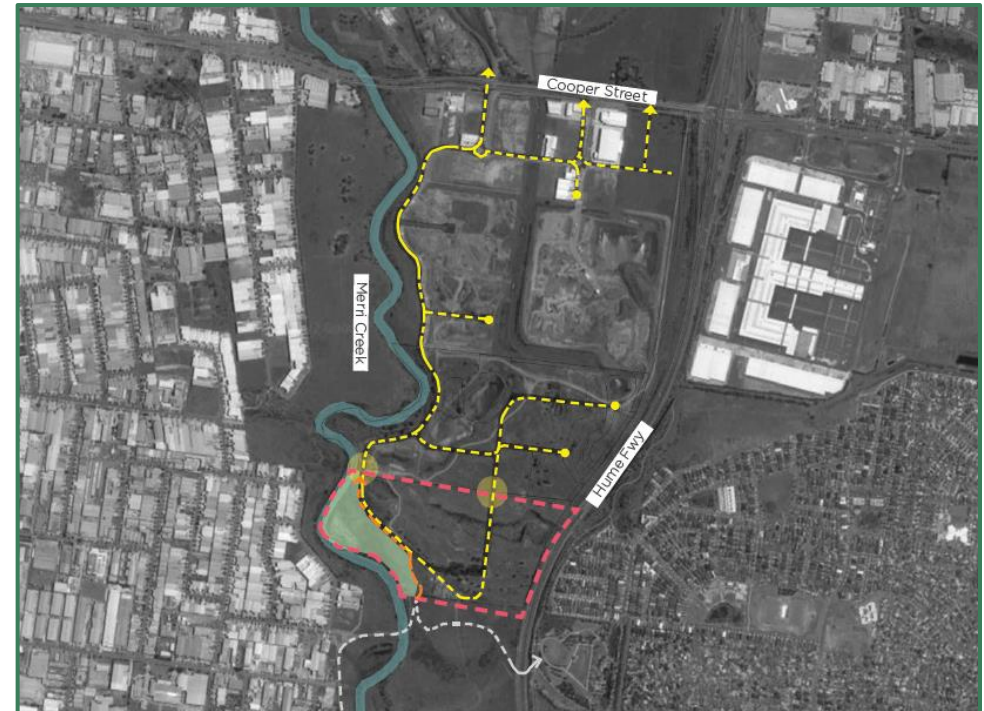


FIGURE 7 | PROPOSED PATH NETWORK

6 Stormwater Management Plan and Drainage Strategy

The Stormwater Management Strategy for the site has been developed and provides a best practice solution within the constraints of the existing landform and proposed development layout. The strategy incorporates an on-site detention system to be provided within the southern portion of the site.

A stormwater quantity and quality management strategy has been developed to reduce both peak flows and pollutant loads in stormwater leaving this site. The Stormwater Management for the development has been designed in accordance with Whittlesea City Council and Melbourne Water's water sensitive urban design requirements.

Notes:

- Sodic and dispersive soils will be considered in the Construction Stormwater Management Plan to ensure the correct controls are implemented, as standard erosion controls, such as silt fences, and conventional sediment controls, such as sediment basins do not work with sodic and dispersive soils.
- The design of the detention and bioretention basins shall enhance biodiversity and amenity values as they provide an interface to the marram baba Parklands.
- The design of the detention and bioretention basins shall consider maintenance and mowing access along the southern boundary (including batter slope) without requiring access from Parks Victoria

land.

- The design of the detention and bioretention basins shall consider maintenance and mowing access along the southern boundary (including batter slope) without requiring access from Parks Victoria land.
- The site is not affected by overland flows. A small part of the site was shown to previously drain north to central creek, however this catchment is minor and overall falls are generally to the south-west and Merri Creek.
- The design of the stormwater system for this site will be based on relevant national design guidelines, Australian Standard Codes of Practice, Whittlesea City Council and accepted engineering practice.
- Overall site runoff and stormwater management will generally be designed in accordance with the Institution of Engineers, Australia publication "Australian Rainfall and Runoff" (1987 Edition), Volumes 1 and 2 (AR&R).

Refer to Appendix 3 for further details.

7 Landscape Concept

The Landscape Concept for the site prioritizes the retention and enhancement of landscaping within the identified Merri Creek Park.

7.1 Landscape Plan

Refer to Appendix 4.

7.2 Proposed Road Cross Sections

The road alignment and design will be consistent with the adjacent site to the north.

- A 23.5-metre-wide road cross section will be provided adjacent to the Merri Creek Park. The 12.2m carriageway must be measured from face to face of the kerbs. The cross-section incorporates a 3-metre-wide shared path along the Merri Creek Park alignment. Refer to Figure 8.
- A 22 metre-wide road cross section will be provided throughout the remainder of the estate. 1.85-metre-wide pedestrian paths will be incorporated on both sides of the road. Refer to Figure 6.

In both road cross-sections the road reserve will incorporate:

- Carriageway - 12.2-metre-wide
- Traffic lanes – single lane in both direction
- Parking – parallel parking on both sides of the road.

Additionally, the Landscape Report provides a cross section of the road layout for the road adjacent Merri Creek. Refer to Figure 8. In areas where the road reserve isn't adjacent Merri Creek both pedestrian footpaths are proposed to be 1.85 metres-wide.

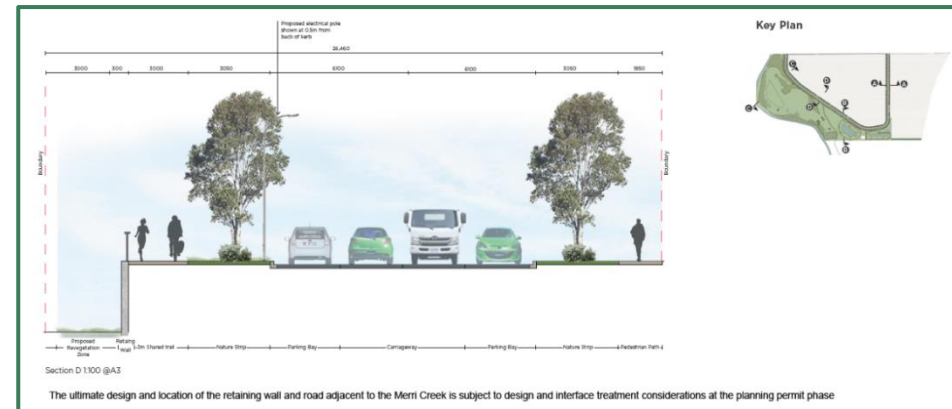


FIGURE 8 - ROAD CROSS SECTION ADJACENT THE MERRI CREEK CORRIDOR

7.3 Land Use and Off-Site Amenity Impacts

Landscaping is to be provided within the front boundary and throughout the car parking areas of the future lots. Refer to Figure 9 for an indicative landscape treatment to property frontages.

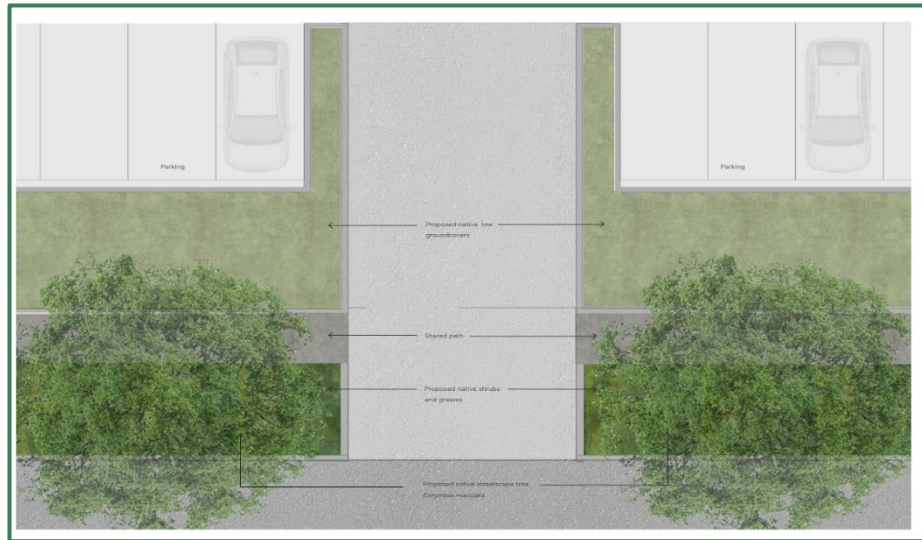


FIGURE 9 - PROPOSED TYPICAL LANDSCAPE FRONTAGE TREATMENT

7.4 Merri Creek Interface Objectives

- Protection of conservation values associated with the Merri Creek corridor, including threatened flora and fauna species.
- Avoidance of impacts on cultural heritage.
- Provision for a 3-metre-wide shared trail.
- Minimise the effect of the height, bulk, and general appearance of any proposed buildings and works on the environmental values and visual character of the creek.

7.5 Other Landscape Details

Additional details including the plant species schedule and Merri Creek interface details are incorporated with Appendix 4.

Notes:

- All paths within the DP area are required to meet DDA requirements.

- Open space to be embellished appropriately with visitor infrastructure by the developer in alignment with City of Whittlesea's Open Space Strategy 2016 and handed over to Council after a maintenance period.
- Subsequent planning application guidelines will include and allow for the following:
 - Proposed habitat crossings (i.e., roads) to be designed in accordance with Growling Grass Frog Crossing Design Standards (DELWP 2017).
 - The lots that interface with proposed Conservation Reserves, Open Space Areas and Habitat Corridors to have a minimum six metre built form setback and drainage to slope away from the interface.
 - The lots that interface with proposed Conservation Reserves, Habitat Corridors and Open Space Areas that share a boundary to be fenced to restrict vehicle access and direct pedestrian movement toward specific access points; and any permeable fence to be small enough to catch litter.
 - That any planted trees are to be sited to minimise shading to retained areas of native grassland and wetlands within Conservation Reserves and Open Space Areas.
 - The requirement for shadow diagrams to accompany any application for use or development of lots adjacent to a proposed Conservation Reserve, Habitat Corridor or Open Space Area.
 - The requirement for building design to minimise shading of any proposed Conservation Reserve, Habitat Corridor or Open Space Area to ensure no overshadowing of grassland or wetland habitat (including any habitat within 30 meters of a waterway or wetland, excluding the proposed stormwater wetland).
 - That lighting is designed and baffled to prevent spill to and

within any proposed Conservation Reserves, Habitat Corridors and Open Space Reserves.

- The requirement for any creation and enhance of Growling Grass Frog habitat to be designed in accordance with the best practice standards for habitat creation, including Melbourne Water Constructed Wetlands Guidelines (2010),
- Growling Grass Frog Masterplan for Melbourne's Growth Corridors (2017) and Guidelines for managing the endangered Growling Grass Frog in urbanising landscapes (2010).
- Open spaces to be embellished appropriately with visitor infrastructure by the developer in alignment with City of Whittlesea's open space strategy 2016 and handed over to Council after a maintenance period.
- Merri creek interface objectives:
 - Protection of conservation values associated with the Merri Creek corridor, including threatened flora and fauna species
 - Avoidance of impacts on cultural heritage
 - Provision of 3 metre wide shared trail
 - Minimise the effect of the height, bulk and general appearance of any proposed buildings and works on the environmental values and visual character of the creek.

Proposed Merri Creek Park Plan



FIGURE 10 - LANDSCAPE CONCEPT AND MERRI CREEK PARK PLAN

9 Cultural Heritage

Summary of Cultural Heritage Report

Standard and Complex Assessments were undertaken on the site with the following findings.

- The desktop assessment identified two previously registered Aboriginal places within the activity area [VAHR 7822-0940 & VAHR 7822-0941]. The site prediction model identified the margins of Merri Creek (<200m) as being of moderate potential for stone artefacts in a subsurface context to a maximum depth of 30cm. All other Aboriginal place types were considered to have no to low potential to be present.

Standard Assessment

- The standard assessment identified the landforms of the activity area as a creek escarpment and volcanic plain. VAHR 7822-0940 & VAHR 7822-0941 could not be relocated. Previous ground disturbances of the activity area include quarrying, recreational use (golf), illegal dumping, informal tracks, and construction of sewer manholes. Two new Aboriginal places were identified and registered as 485 Cooper Street AS [VAHR 7822-XXXX] and 485 Cooper Street LDAD [VAHR 7822-4798]. Ground surface visibility was very poor and effective surface coverage was less than 10%. Due to the activity area deemed as likely to contain Aboriginal Cultural Heritage based on the standard assessment, a complex assessment was undertaken.

Complex Assessment

- The complex assessment consisted of two 1x1m controlled and manually excavated TPs and 91 5x1m mechanical trenches. Total

excavation is 455m² with 1m² containing Aboriginal cultural heritage. Mechanical trench 25, 26, 62, 63, 64 & 65 were abandoned due to asbestos. The stratigraphic profile identified was silty clay ranging from weak to strong over clay. The soil profile encountered within the TP is consistent with the predicted stratigraphic profile for this landform. Modern debris and fill include were located within 64.8% of mechanical trenches.

Management of Cultural Heritage

- Discussions have taken place with the RAP and approval has been granted for ground salvage of the known artefact scatters identified on the site.

10 Geotechnical

Refer to Appendix 7 and 8 for information on the Site Contamination and Geotechnical assessments. The appendices include:

- An analysis and plan of the existing geotechnical conditions, including areas of cut and fill;
- Identification of any areas of previous excavation or quarrying;
- Potentially Contaminated Land, Preliminary Sie Investigation; and
- An environmental assessment identifying any environmental hazards or contamination of the land and proposed treatments, if any; or a qualified statement indicating the absence of such hazards or contamination.

- Contamination
 - The site is not likely to be contaminated to a level which would pose a significant risk to the environment or human health under the proposed use/development scenario. No further assessment is required.

11 Conclusion

This Development Plan has been prepared in accordance with the relevant requirements of the Development Plan Overlay.

The Development Plan is considered to align with the objectives of Development Plan Overlay – Schedule 33 and is supported by the relevant expert assessments which are included as appendices.

The Development Plan balances the objectives of the Industrial 1 Zone while also protecting and enhancing the environmental characteristics of the site's Merri Creek interface. This document will guide the future subdivision and building and works application for the site.

Appendix 1 – Development Plan (SBA Architects, 19/06/2024)

Appendix 2 – Transport Impact Assessment (Impact Australia, 19/10/2023)

Appendix 3 - Stormwater Management Plan (Costin Roe Consulting, 20/06/2024)

Appendix 4 – Landscape Planning Report (Outerspace Landscape Architects, 19/12/2023)

Appendix 5 – Arborist Report (Arbor Survey, 08/09/2022)

Appendix 6 –Flora and Fauna Assessment (Nature Advisory, 21/06/2024)

Appendix 7 – Geotechnical Assessment (Industrial Park) (AS. James, 20/07/2023)

Appendix 8a – Soil Contamination Assessment (Edge Group, 06/09/2022 & 22/11/2022)

Appendix 8b – Soil Contamination Assessment (Edge Group, 22/11/2022)

Appendix 8c – Environmental Preliminary Site Investigation Cover Sheet (completed by Helia EHS Pty Ltd (formally Edge Group Pty Ltd, 9/10/2023)

Appendix 9 – Planning Report (proUrban Advisory, Planning and Management, 21/06/2024)

Appendix 10 – Concept Servicing & Staging Report (Costin Roe Consulting, 22/06/2024)

Appendix 11 – Site Context Plan (SBA Architects, 20/07/2023)

Appendix 12 – Habitat Plan (Nature Advisory June 2023)

Appendix 13 – Cultural Heritage Report (Tardis Archaeology Heritage Advisors 19/01/2024)